

**22.06 RINGWOOD ACTIVITY CENTRE**14/01/2016  
C95

This policy applies to all land in the ‘Activity Centre’, the ‘Western Gateway Precinct’ or in a ‘Residential Development Area’ as shown on the Ringwood Activity Centre locality plan that forms part of this policy.

**22.06-1 Policy basis**11/07/2014  
C91

This policy builds on the MSS objective in clause 21.04 to promote design and development that encourages pedestrian activity and where possible, increases the permeability of our suburbs.

This policy builds on the MSS objective in clause 21.07 to ensure the ongoing development of a quality and attractive built environment based on sound planning and consistent urban design principles, to protect and enhance the visual amenity of residential, commercial and industrial precincts and streetscape and to encourage development that provides and creates high quality urban places.

This policy builds on the MSS objective in clause 21.09 to ensure the successful operation of retail and commercial centres in Maroondah through sustainable planning and economic development.

**22.06-2 Ringwood Activity Centre**14/01/2016  
C95

This clause applies to all land identified as being within the ‘Activity Centre’ on the Ringwood Activity Centre locality plan that forms part of this policy.

**Land use and activity****Objectives**

To establish the Ringwood Activity Centre as the premier activity centre within the outer east of Melbourne, reinforcing its role as the principal retail, commercial, community, entertainment and employment focal point of the region.

To encourage a range of commercial, civic and residential land uses, including higher density housing, that build on the existing and planned infrastructure of the Ringwood Activity Centre and provide the opportunity for multi-purpose trips.

To encourage integration of public transport and land use planning.

To encourage the consolidation of small fragmented allotments in order to create greater opportunities to intensify the level of all forms of development within the Activity Centre.

**Policy**

It is policy to:

- Intensify the level of activity within the centre through encouraging a diverse mix of residential, civic and commercial development at an increased density.
- Strengthen the retail and commercial role of the Activity Centre through encouraging a consolidated and expanded retail core embracing Eastland, the Town Centre and the linked areas of Ringwood Street and the Ringwood Market.
- Encourage commercial areas outside the Town Centre to diversify to include a wide range of business and employment opportunities to facilitate Ringwood’s role as a Metropolitan Activity Centre.

- Provide view lines into the Town Centre through the creation of physical and visual links to adjoining precincts.
- Recognise Ringwood Lake as a major regional recreation destination with appropriate internal pedestrian and cyclist links from within and outside the Activity Centre.
- Ensure residential development north of Bond Street include landscaping on private land that complements the existing indigenous landscaping along the Mullum Mullum Creek.
- Develop Maroondah Highway with high quality large format showroom uses serviced from the rear.
- Strengthen and improve pedestrian linkages, amenity and access throughout the Activity Centre.
- Ensure development facilitates active frontages to streets, walkways and public spaces.

### **Built form and scale**

#### **Objectives**

To define an urban form that provides a safe and attractive environment through clear physical linkages, increased lighting, active retail and commercial uses at street level and legible movement systems.

To encourage innovative building and public domain design that contributes to the diversity of the Ringwood Activity Centre that respects and includes its existing built assets and image defining buildings and responds to its topographical and site specific conditions.

To encourage excellence in architectural and building design throughout the Activity Centre.

To encourage the development of landmark and feature buildings that by exceeding the general building scale strongly define the Ringwood Town Centre, key junctions, topographical high points and gateways to and within the Activity Centre.

To encourage development that creates a sense of place, safety and public ownership of common areas.

To create a strong visual connection between the Ringwood Activity Centre's transport and commercial hubs and visually define the Activity Centre's edges and gateways to improve the image and legibility of the centre.

To encourage innovative, high quality architecture and streetscape design that incorporates principles of Environmentally Sustainable Design, Crime Prevention through Environmental Design and Water Sensitive Urban Design that sets a benchmark for future design standards across the whole municipality.

#### **Policy**

It is policy to:

- Design new development to create a sense of address with active, open, attractive and pedestrian friendly interfaces to the street.
- Ensure the redevelopment of existing surface car parking areas incorporate underground or attractive integrated deck parking with 'active' frontages.
- Establish consistent built form, built to the street frontage on either side of the road along all major roads within the Activity Centre to create the sense of a boulevard.

- Consolidate fragmented land parcels throughout the Activity Centre to create opportunities to intensify development and to clearly distinguish the Activity Centre from the low-rise suburban surrounds.
- Integrate well designed high density apartment style housing into the residential precincts east of Warrandyte Road and west of Ringwood Street, as well as the Commercial Boulevard and the Ringwood Town Centre.
- Ensure new development address the following sustainability issues, to the satisfaction of the responsible authority:
  - Building design (including energy efficiency and energy saving technologies).
  - Water sensitive urban design (including water conservation features and stormwater management).
  - Construction materials (including use of materials that minimise environmental impacts and use of recycled materials).
  - Indoor environment quality (including internal lighting and air quality, minimising noise levels and noise transfer between buildings and external areas).
  - Waste management (including minimising waste and maximising recycling/composting opportunities).
  - Transport (design of car parking areas to reduce energy consumption and incorporation of 'after-trip' facilities for cyclists and joggers).
  - Demolition and construction (including the protection of vegetation and other public assets identified for retention, minimising building waste, prevention of off-site pollutants, minimising noise and off-site amenity impacts, minimising disruption to adjacent properties and maintaining vehicular and pedestrian access).

## **Landscape and environment**

### **Objectives**

To use the natural environmental features of the Ringwood Activity Centre, such as the landscape and topography, to distinguish the Activity Centre from other suburban activity centres.

To integrate Environmentally Sustainable Design principles and techniques into the design, construction and operation stages of new development to recognise and contribute towards a long-term, responsible and sustainable future for the Ringwood Activity Centre and the wider community.

### **Policy**

It is policy to:

- Establish high quality landscape settings that reflect topography and landscape themes and complement the existing environmental features such as the Mullum Mullum Creek and Ringwood Lake.
- Ensure high quality public domain, landscape and environmental features be implemented into the Activity Centre to distinguish Ringwood from other activity centres across Metropolitan Melbourne.
- Incorporate environmentally sustainable design principles into the design, construction and operation stages of all new development.

## Access and movement

### Objectives

To establish Ringwood Activity Centre as a convenient, safe, and accessible place to live, work and shop for pedestrians, cyclists, public transport users and car based travellers.

To reduce the emphasis on through traffic to provide for regeneration of the Maroondah Highway as the primary Activity Centre streetscape with high levels of pedestrian amenity and forming an inviting corridor leading towards a more intense Town Centre focus.

To create a strong visual connection between the Ringwood Activity Centre's transport and commercial hubs and visually define the Activity Centre's edges and gateways to improve the image and legibility of the centre.

To strengthen the role of Ringwood Station as the primary modal interchange for the region.

To provide legible access and car parking that does not dominate or compromise pedestrian and public transport movement in the Ringwood Activity Centre.

To provide for safe, convenient and attractive pedestrian and cyclist links within the Activity Centre and to surrounding residential areas.

To strengthen and improve pedestrian linkages, access, and amenity throughout the Activity Centre.

### Policy

It is policy to:

- Ensure all precincts be accessible for all forms of transport.
- Upgrade public transport facilities, including Ringwood Station and the Bus Interchange to provide a competitive and attractive alternative to private vehicle use.
- Extend Charter Street east past the Ringwood Market and be recreated as the preferred active east-west link through the Activity Centre.
- Establish a series of pedestrian and bicycle links to allow safe and convenient movements throughout the Activity Centre.
- Ensure new access and connections be built at-grade in order to encourage intensification of activity at street level.

## 22.06-3

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### Town centre

This clause applies to all land identified as being within the 'Town Centre' on the Ringwood Activity Centre locality plan that forms part of this policy.

### Objectives

To establish a consolidated urban Town Centre within the Ringwood Activity Centre with strong functional and visual links to the surrounding Activity Centre precincts.

To promote a balanced land use configuration that will complement adjacent activities and reinforce and highlight the role of the Ringwood Town Centre as a key component of a metropolitan transit oriented centre.

To encourage a range of retail, commercial, civic and residential land uses, including higher density housing, that maximise the existing public transport infrastructure and provide for activity in the Ringwood Town Centre across a range of hours.

To provide for the development of the built form that highlights key Town Centre landmarks, vistas and gateways.

To ensure the height of landmark 'feature' buildings complement the public and private realm and contribute in defining the Ringwood Town Centre as a destination.

To ensure urban development contributes to a highly visible and unique Town Centre focus and community meeting place that respects and enhances its existing built assets and image defining buildings.

To emphasise convenient and safe access between the transport interchange and the Ringwood Town Centre.

To provide for the development of commuter car parking to the level appropriate to a regional transport and commercial hub.

To integrate a safe and efficient bus interchange with direct connections to rail, taxi, and commuter parking facilities and neighbouring commercial destinations.

## **Policy**

It is policy to:

- Redevelop the Ringwood Railway Station and environs into an integrated transport interchange which is to provide for:
  - A transit hall.
  - Bus interchange facilities.
  - Commuter parking.
  - Commuter shelter.
  - Practical, attractive, safe and convenient access between Station Street and the Maroondah Highway and between the different levels and components of the transport interchange for people of all mobility levels.
  - A broad, attractive and safe at-grade pedestrian crossing of the Maroondah Highway adjacent to the public transport interchange
- Provide a broad physical and visually active link from the public transport interchange area, through the public areas in the heart of the Town Centre north of Maroondah highway, and leading to the Eastland south entry.
- Discourage the development of uses that are solely car based and convenience style uses that contain "drive thru".
- Ensure a visible and active civic and retail presence be formed close to the Maroondah Highway, with active and open retail street frontages encouraged along both sides of Maroondah Highway to strengthen the Town Centre east-west spine.
- Encourage active and open retail street frontages along Ringwood Street, Warrandyte Road and abutting public spaces within the town centre area, to create a sense of street address and streetscape appeal, encourage passive surveillance, provide attractive and pedestrian-friendly interfaces, and allow for integration with the public realm.
- Create a spacious, green town square in the central Town Centre area that provides direct visual and physical links to the Ringwood Station forecourt, Maroondah Highway and the areas to the west of Ringwood Street.
- Ensure the design and form of the Town Square allows for the area to function both formally and informally as a meeting place for events, community activities and passive recreation.
- Encourage the creation of landmark multi-storey buildings on specific key sites to create a unified and defining presence for the Town Centre.

- Encourage the establishment of prominent destination uses in the feature buildings of the Ringwood Town Centre .
- Include library, educational and civic facilities in the Town Centre.
- Encourage the consolidation of fragmented land parcels to facilitate a higher intensity of built form within the Town Centre.
- Define a meaningful and dynamic role for the Clock tower and Memorial Park on the corner of Wantirna Road and Maroondah Highway as a key gateway to the Town Centre.

**22.06-4 Commercial boulevard**

11/07/2014  
C91

This clause applies to all land identified as being within the ‘Commercial Boulevard Areas’ on the Ringwood Activity Centre locality plan that forms part of this policy.

**Objectives**

To ensure that core and traditional retailing activities are consolidated within the Business 1 Zone.

To promote the development of high quality large format showroom, office and restricted commercial activities within the Business 2 Zone along Maroondah Highway in the Ringwood Activity Centre Commercial Boulevard.

To ensure the development of buildings assists in the creation of a boulevard effect.

To establish apartment style residential housing within appropriate Commercial Boulevard sites.

**Policy**

It is policy to:

- Ensure retailing development reinforces the established pattern and hierarchy of activity in the Ringwood Activity Centre in which shops, offices, public and community services and entertainment facilities are grouped.
- Encourage office development to locate along the Ringwood Activity Centre Commercial Boulevard.
- Encourage large format showroom uses along Maroondah Highway.
- Encourage the development of restricted retail premises.
- Support apartment style residential housing in appropriate Commercial Boulevard sites provided this site includes office and/or restricted commercial activity.
- Encourage buildings either side of Maroondah Highway of a consistent scale and built hard to the street frontage to achieve a coherent boulevard effect.

**22.06-5 Western gateway**

11/07/2014  
C91

This policy applies to all land identified as being within the ‘Western Gateway’ area on the Ringwood Activity Centre locality plan that forms part of this policy.

**Objectives**

To protect the significant panoramic views towards the Mount Dandenong Ranges and Wicklow Hills from the entry to the municipality.

To provide for the development of built form that highlights the western gateway of the Ringwood Activity Centre.

To ensure the height of gateway 'feature' buildings complement the public and private realm.

To emphasise convenient and safe access between public transport services and the various destinations within this precinct.

To improve pedestrian accessibility of the precinct.

To continue the strong landscape theme and character of Whitehorse Road (Maroondah Highway) to the west.

## **Policy**

It is policy to:

- Maintain the strong view lines experienced along Maroondah Highway to the east toward the Mount Dandenong Ranges ridgeline .
- Encourage landmark multi-storey buildings set above a podium on Maroondah Highway immediately adjacent to and east of the Eastlink Interchange subject to impacts on views to the Dandenong Ranges and Wicklow Hills ridge being maintained.
- Ensure views to the Wicklow Hills and Dandenong Ranges ridgelines are maintained with built form along Maroondah Highway responding to sloping topography and providing recessed upper levels.
- Ensure built form along Maroondah Highway incorporate active frontages and attractive principal exposed edges.
- Setback built form at 8-12 Maroondah Highway from the street to allow for landscaping consistent with the existing character of Whitehorse Road/Maroondah Highway to the west.
- Provide safe pedestrian and disabled persons access along Maroondah Highway to allow connections between Heatherdale Station, commuter parking and the Ringwood Activity Centre.
- Ensure street tree planting along Maroondah Highway to the east be restored with suitable and consistent species.
- Encourage the development of large quantities of car parking and asphalted areas for higher purposes .
- Locate any proposed at-surface car parking areas to the sides and rear of sites.
- Encourage at-surface car parking areas to include adequate canopy and shrub landscaping and defined pedestrian paths and directional signage within all car parking areas.

### **22.06-6**

11/07/2014  
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## **Residential development**

This clause applies to all land identified as being within 'Residential Development Areas' on the Ringwood Activity Centre locality plan that forms part of this policy.

## **Objectives**

To encourage and provide for the development of multi level, multi occupancy apartment style residential buildings as the preferred built form in the Ringwood Activity Centre Residential Development Policy area.

To promote the sympathetic and managed development of the preferred residential precincts with multi level residential buildings.

To ensure that multi level, multi occupancy residential buildings are designed and developed to complement and enhance the form and regional role of the Ringwood Activity Centre.

To ensure that residential development contributes to the establishment of the preferred neighbourhood character.

To ensure that residential development is designed and constructed to a high standard of visual appearance and resident amenity and makes a positive contribution to the preferred neighbourhood character.

To encourage innovative medium and high density housing design based on integrated, multi level and multi occupancy forms of development on consolidated lots.

To enable increased housing density and building heights in locations with convenient access to public transport and commercial areas that are well designed and articulated buildings with innovative and contemporary architectural responses.

To create larger lot sizes through the consolidation of existing smaller lots and ensure development potential is fulfilled on larger sites while respecting the valued character of the Precinct.

To ensure an appropriate amount of parking is provided on-site with new developments and improve access to on-street parking.

To achieve a consistent and minimal front boundary setback, and to establish the desired spatial proportions of the street and define the street edge to ensure cohesive and consistent streetscapes.

To provide a high standard of residential amenity to the occupants of the building and to neighbouring properties, and to facilitate the retention of canopy trees.

To maximise solar access and improve the thermal efficiency of buildings, while providing an address to the street.

To reduce the visual dominance of the upper levels of buildings when viewed from street level.

To accentuate street corners throughout the area, and reinforce the street hierarchy.

To enhance the security of properties, and to achieve a consistent and attractive pedestrian environment that:

- optimises the safety of building entrances,
- maximises opportunities for casual surveillance from and within new developments, and
- minimise opportunities for concealment near and within new developments.

To ensure that any communal open space is designed to be usable and attractive and to contribute to the quality and amenity of communal open space, and to encourage the establishment and healthy growth of vegetation in urban areas.

To contribute to the streetscape character and amenity of the public domain, and to provide a pleasant outlook for residents.

To retain the treed character of the area and discourage the loss of existing canopy trees.

To encourage consideration of the microclimate and solar performance of the development.

To minimise the impacts of new development on natural waterways, and to encourage the integration of water saving measures in their design.

To integrate the location and design of car parking with the design of the site and building(s).



To ensure that underground parking facilities and access do not dominate the building or streetscape.

To encourage new developments to provide an outlook towards the Mullum Mullum Creek, and provide casual surveillance of the adjacent parkland.

To encourage well designed buildings that relate to the character of the Mullum Mullum Creek environment.

To reinforce the landscape character of the Mullum Mullum Creek environs.

To enhance the interface between new development and the public parkland adjacent to the Mullum Mullum Creek.

## **Policy**

It is policy to:

### **Neighbourhood character**

- Ensure residential development consists of multi level, multi occupancy buildings on consolidated lots.
- Provide for multi level, apartment style residential buildings that retain elements of the existing garden setting. Buildings will be larger apartment style, single buildings constructed on consolidated sites.
- Provide for a higher intensity of site development than occurs at present.
- Recognise the existing street pattern and create buildings that form visual landmarks throughout the precincts.
- Ensure development will contribute to the preferred neighbourhood character by:
  - Providing strategic opportunities for the planting or retention of canopy trees, particularly at the front of buildings, to maintain the existing streetscape and frame larger buildings.
  - Ensuring that the building form retains a human scale and is designed to avoid large, block like structures dominating the streetscape.
  - Providing a mix of building forms and heights that generally accord with the Ringwood Activity Centre indicative building height map.
  - Consolidating sites in a logical and progressive manner that avoids the creation of isolated lots of limited redevelopment potential.
  - Providing the opportunity to enhance pedestrian activity and contribute to the creation of a sense of place.
  - Relating building height to lot size.
  - Limiting vehicle crossings to 1 per site and providing common access to sites.

### **Site consolidation**

- Ensure the consolidation and development of sites occurs in a progressive manner and does not result in creation of small, isolated holdings of land of limited development potential.
- Ensure multi level, multi occupancy development is constructed on consolidated sites.
- Discourage developments that further fragment existing land holdings .

### **Land use**

- Not support non-residential use and development in the Ringwood Activity Centre Residential Development area, except where designed and proposed as part of an integrated residential development of the land.

### **Building height**

- Ensure building heights for residential development located within the area identified in the Ringwood Activity Centre – Indicative Building Height Plan relate to the following principles:
  - 3 storey development on lots generally greater than 1000m<sup>2</sup>.
  - 4 storey development on lots generally greater than 2500m<sup>2</sup>.
  - 4 storeys and higher on lots generally greater than 3000m<sup>2</sup>.
- Ensure building heights minimise the impact of overshadowing and the impression of building mass and bulk on adjacent residential buildings.

### **Footpaths**

- Provide footpaths.

### **Front setback**

- Reduce the front setback on consolidated sites that are providing for the development of multi level, multi occupancy residential buildings.
- Ensure the front setback of a residential building provides for visual and physical transition between the streetscape and the development.
- Ensure front setbacks be planted with substantial tree cover to ‘soften’ the impact of new buildings on the streetscape.

### **Vehicle crossovers**

- Limit the number of vehicle crossovers to maintain the integrity and continuity of the streetscape.
- Ensure vehicle crossovers are located to prevent traffic disruption, maximise access and egress opportunities and ensure the preservation of streetscape features such as street trees.

### **Vehicle access driveways**

- Ensure vehicle access driveways, which allow vehicle access to more than one (1) dwelling, are provided as common property on a plan of subdivision.
- Provide vehicle access driveways with a form and material that assists in softening the visual and physical dominance of this element in the design of the residential development.
- Design and locate residential buildings to minimise the length of the vehicular access driveway that is visible between the street frontage and the undercover car parking area.

### **Setback from public areas**

- Design buildings adjacent to public open space areas and pedestrian accessways between streets to minimise the overpowering of the public areas and retain a human scale.
- Design and locate residential buildings to prevent the creation of a “tunnel” effect on any area of public open space or public accessway.

### **22.06-7**

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### **Policy references**

Ringwood Transit City Urban Design Masterplan, Hansen Partnership, 2004

Ringwood Transit City North West Residential Precinct Plan, Planisphere, June 2009

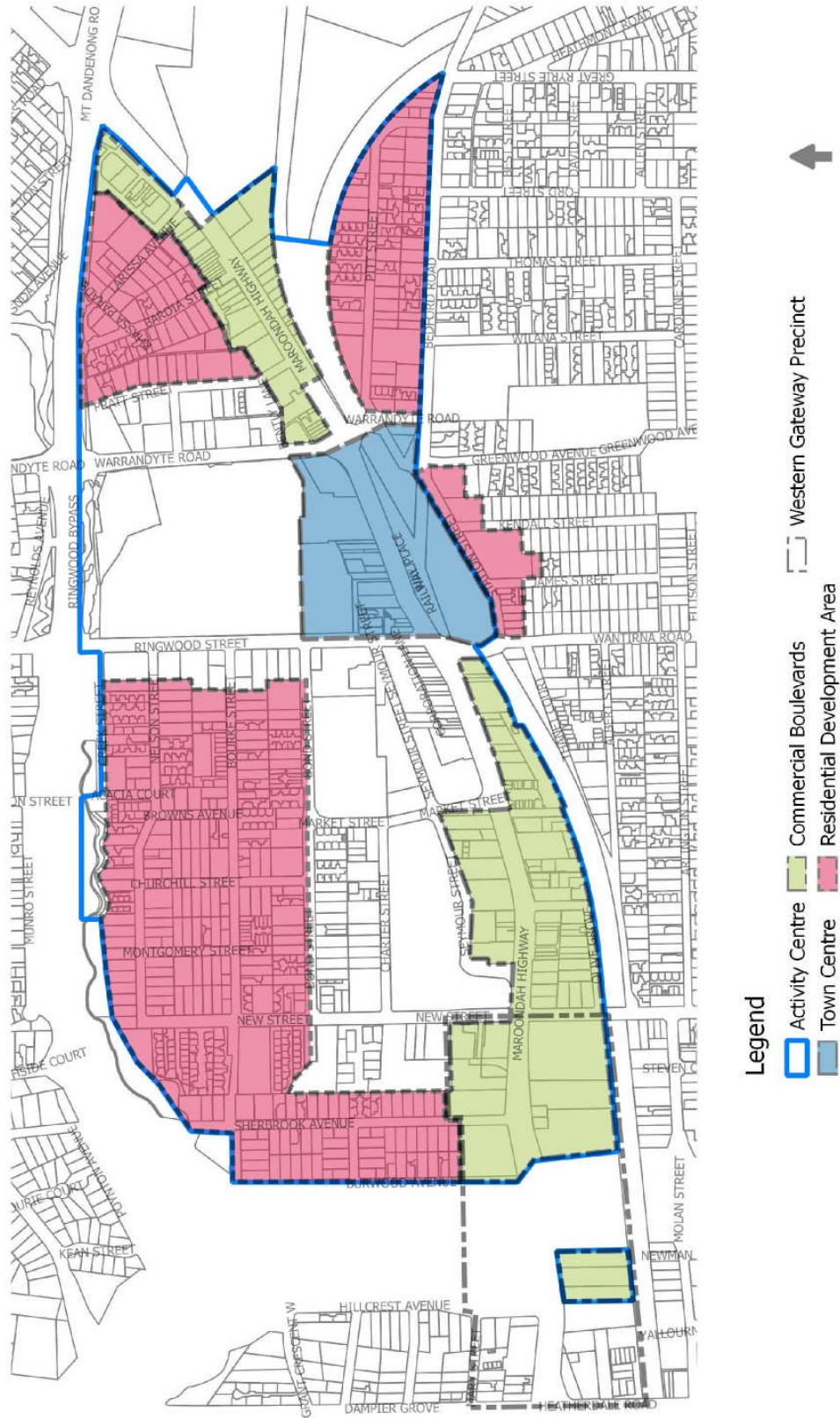
Ringwood Western Gateway Urban Design Review, Hansen Partnership, 2007

Ringwood Transit City Public Domain and Landscape Guidelines, Hansen Partnership, 2006

Maroondah Residential 2 Zone Urban Design Frameworks & Guidelines, Murphy Design Group & Fulcrum Town Planners, 2000

Sites of Biological Significance in Maroondah Vols 1 & 2, G.S. Lorimer, J.C. Reid, L.P. Smith, and H. Moss, 1997

Ringwood Activity Centre Locality Plan



**Ringwood Activity Centre – Indicative Building Height Plan**

